

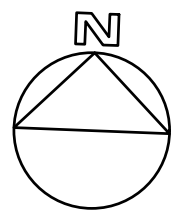
BARTLETT CITY SCHOOLS PAVEMENT FULL DEPTH RECLAMATION BARTLETT ELEMENTARY SCHOOL

A2H ENGINEERS
ARCHITECTS
PLANNERS



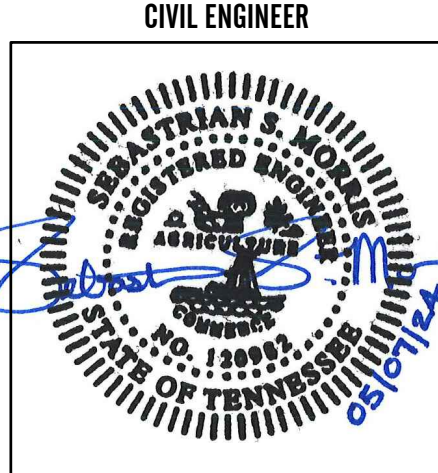
PROJECT LOCATION

VICINITY MAP
1" = 1000'



**5705 STAGE ROAD
BARTLETT, TN 38134**

**CONSTRUCTION DOCUMENTS
MAY 7, 2024
PROJECT NUMBER 24218**



A2H, INC.
3009 DAVIES PLANTATION ROAD
LAKELAND, TN 38002
P. 901.372.0404
WWW.A2H.COM

INDEX OF SHEETS

DESCRIPTION	SHEET NO.
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NOTE: THE CHARACTERS (LETTERS) I, O, AND Q ARE NOT USED IN NUMBERING OF SHEETS.

A1	SHEET INDEX
NTS	

STANDARD ROADWAY DRAWINGS

STANDARD ROADWAY TITLE SHEET, ABBREVIATION, AND LEGENDS

DWG. NO	REV.	DESCRIPTION
RD-A-1	02/20/2000	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02/20/2020	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

ROADWAY, PAVEMENT APPURTENANCES, AND FENCES

DWG.	NO	REV.	DESCRIPTION
RP-VC-10	03/04/2021		VERTICAL CONCRETE CURBS AND CURB AND GUTTER (FOR 8" TO 12" GUTTER DEPTH)

EROSION PREVENTION AND SEDIMENT CONTROL

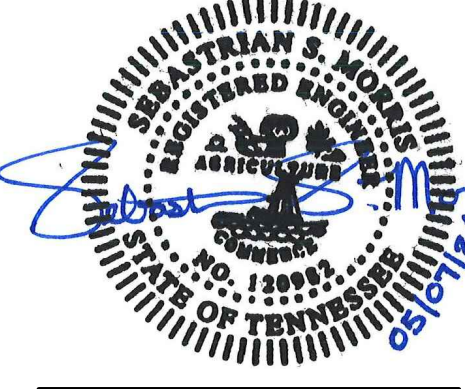
DWG. NO	REV.	DESCRIPTION
EC-STR-39A	08/01/2012	CURB INLET PROTECTION TYPE 3 & 4

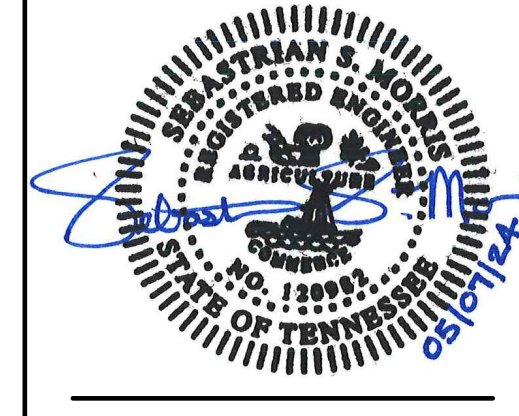
B1	STANDARD ROADWAY DRAWINGS
NTS	

D1	INDEX & QUANTITIES
NTS	

ESTIMATED ROADWAY QUANTITIES				BARTLETT ELEMENTARY	
FOOT-NOTES	ITEM NO.	DESCRIPTION	UNIT	QUANTITY	
	202-01	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	LS	1	
1, 2	203-01.05	EXCAVATION (FDR AREAS)	CY	650	
	209-05	SEDIMENT REMOVAL	CY	75	
	209-09.43	CURB INLET PROTECTION (TYPE 4)	EACH	6	
	304-01.04	PROCESSING (RECLAIMED BASE MATERIAL)	SY	9,220	
7	304-01.08	PORTLAND CEMENT (FULL DEPTH PAVEMENT RECLAMATION)	TON	416	
	307-01.07	ASPHALT CONCRETE MIX (PG 64-22) (BPMB-HM) GRADING B-M	TON	707	
6	309-02	BITUMINOUS MATERIAL (A-CBC)	TON	9	
	403-01	BITUMINOUS MATERIAL FOR TACK COAT	TON	3	
	407-20.05	SAW CUTTING ASPHALT PAVEMENT	LF	2,205	
	411-01.11	ACS MIX (PG 64-22) GRADING E ROADWAY	TON	1,045	
2	415-01.02	COLD PLANING BITUMINOUS PAVEMENT	SY	644	
3	702-01	CONCRETE CURB	CY	47	
4	702-01.03	CONCRETE CURB	LF	640	
	702-10.02	WHEEL STOP	EACH	10	
5	716-03.09	PLASTIC WORD PAVEMENT MARKING (FIRE LANE)	EACH	2	
5	716-04.10	PLASTIC PAVEMENT MARKING (HANDICAP SYMBOL)	EACH	6	
	716-05.08	PAINTED PAVEMENT MARKINGS (PARKING LINE)	LF	3,105	
	716-05.50	PAINTED PAVEMENT MARKINGS (8" LINE)	LF	782	
	717-00	MOBILIZATION	LS	1	
	920-10.05	CONSTRUCT 20 L.F. OF 2' WIDE CONCRETE SWALE	LS	1	

1. EXCAVATION TO INCLUDE ALL MATERIAL NECESSARY TO ATTAIN PROPER DEPTH.
2. CONTRACTOR TO HAUL OFF ALL MILLED AND EXCAVATED MATERIAL FOR DISPOSAL AT APPROPRIATE AND PERMITTED SITE. SEE SHEET 2: TYPICAL SECTIONS.
3. TOP OF TDOT RP-VC-10 6" DETACHED CONCRETE CURB TO MATCH SIDEWALK ELEVATIONS. SEE PROPOSED LAYOUT FOR LOCATIONS.
4. FACE OF GUTTER TO MATCH EDGE OF ASPHALT. SEE PROPOSED LAYOUT FOR LOCATIONS.
5. CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
6. NO TRAFFIC FOR 7 DAYS. CONTRACTOR HAS THE OPTION OF KEEPING THE FDR BASE DAMP WITH WATER TRUCK FOR 7 DAYS. IIF THE CONTRACTOR CAN INSTALL THE SURFACE ASPHALT WITHIN 24 HOURS AFTER PROCESSING TRAFFIC CAN BE ALLOWED.
7. SPREAD CEMENT AT A RATE OF 52 LB/SY.
8. ALL WORK SHALL FOLLOW TDOT STANDARS AND SPECIFICATIONS.





BARTLETT CITY SCHOOLS

PAVEMENT FULL DEPTH RECLAMATION

5705 STAGE ROAD
BARTLETT, TN 38134

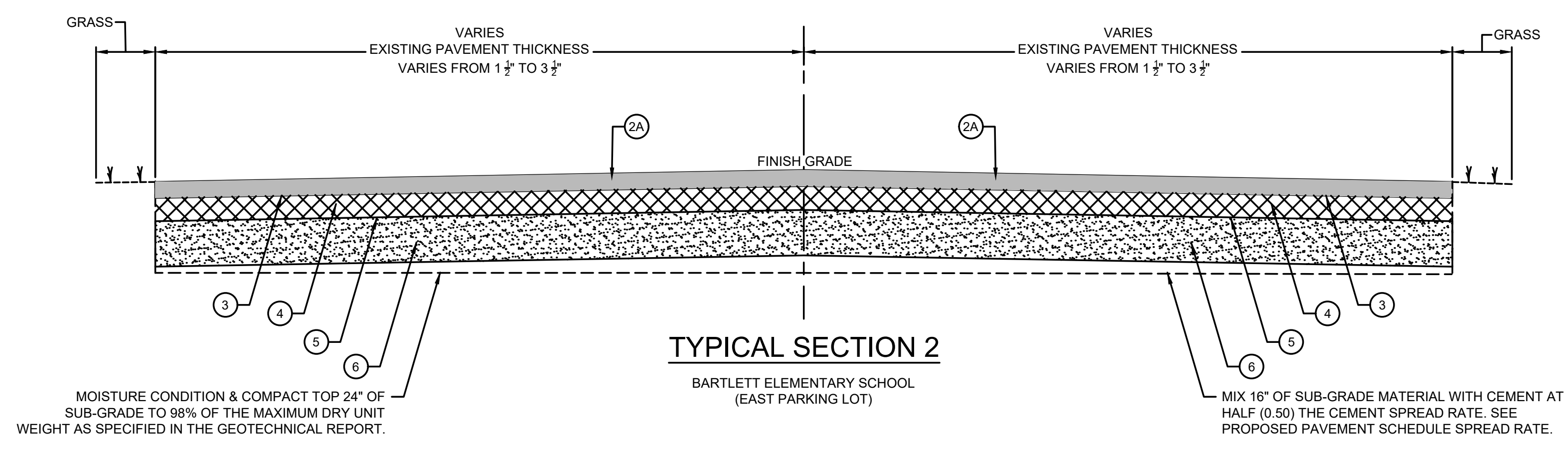
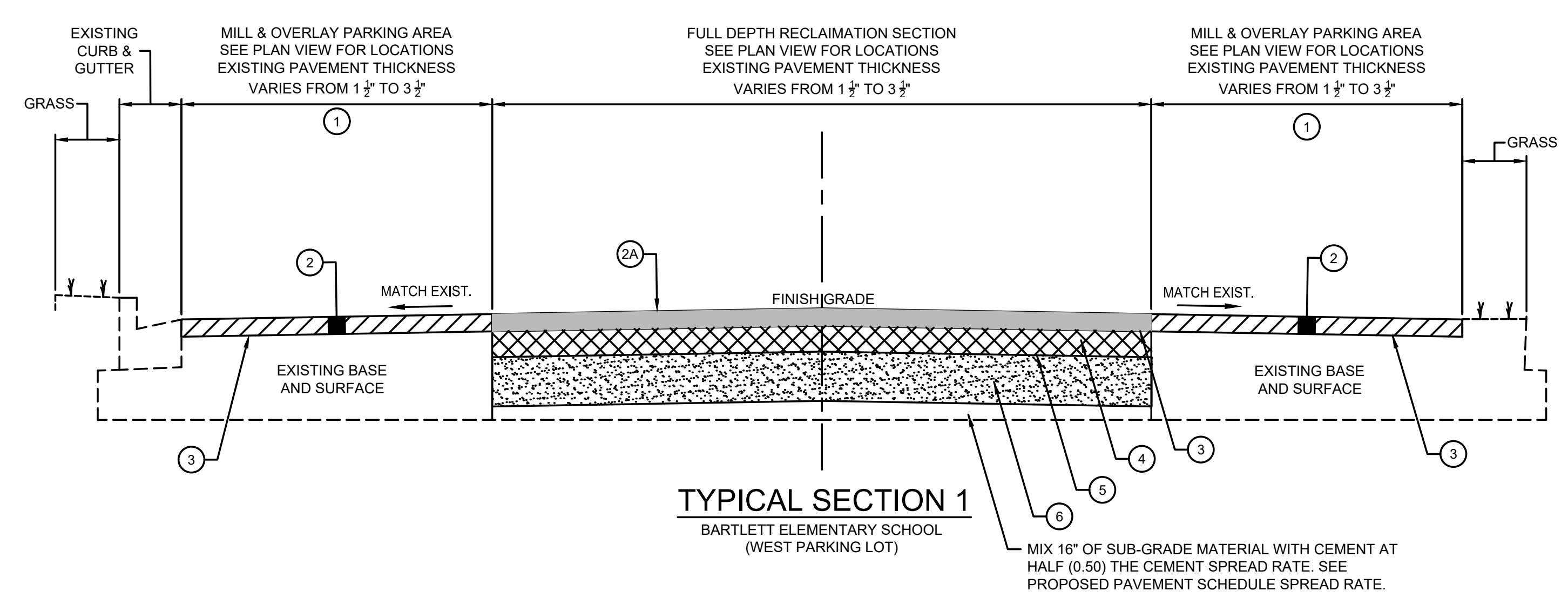
CONSTRUCTION DOCUMENTS

REVISIONS

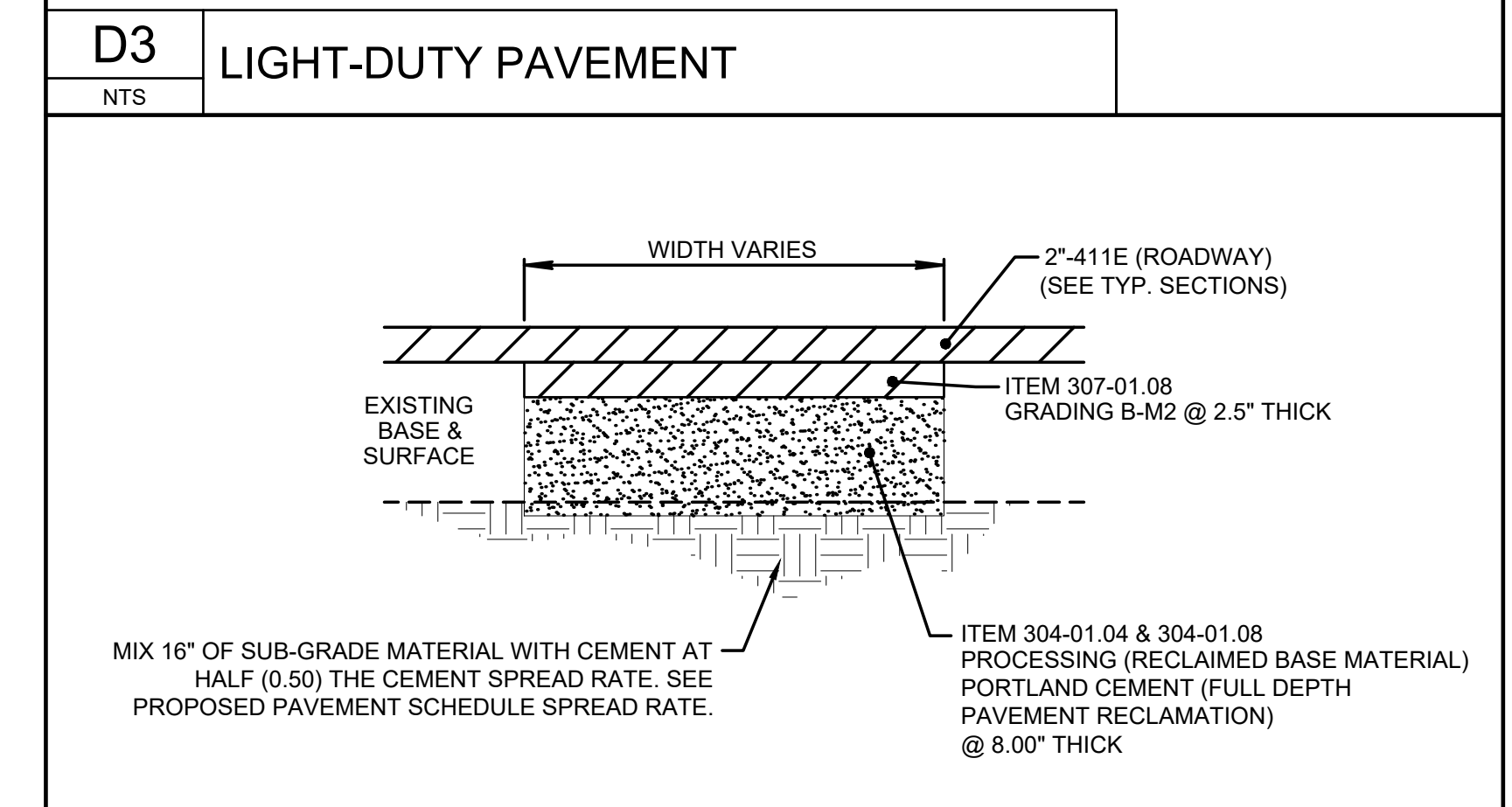
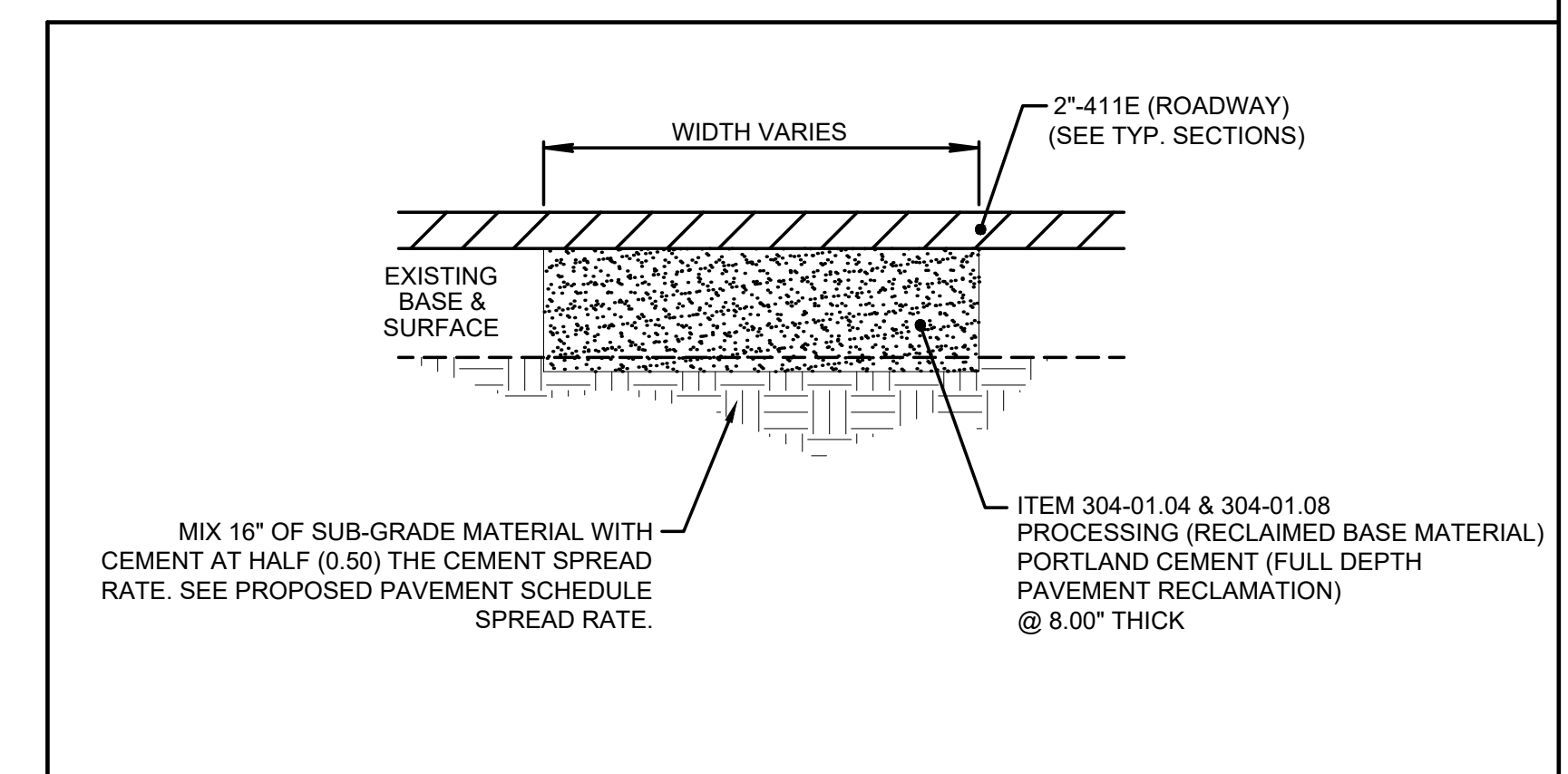
PROJECT NO. 24218 DATE MAY 7, 2024

DRAWN SWJ CHECKED SSM

TYPICAL SECTIONS



PROPOSED PAVEMENT SCHEDULE		
①	COLD PLANING @ 1.50" DEPTH ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT	APPROX. 157.50± LB/S.Y.
②	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.50" THICK ITEM 411-01.11 ACS MIX (PG64-22) GRADING E ROADWAY	APPROX. 159.00± LB/S.Y.
②A	ASPHALTIC CONCRETE SURFACE (ACS) @ 2.00" THICK ITEM 411-01.11 ACS MIX (PG64-22) GRADING E ROADWAY	APPROX. 159.00± LB/S.Y.
③	TACK COAT ITEM 403-01 SEE TDOT ROADWAY DESIGN GUIDELINES ITEM 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.	APPROX. 231± GAL/TON
④	BITUMINOUS PLANT MIX BASE @ 2.50" THICK ITEM 307-01.07 ACS MIX (PG64-22) (BPMB-HM) GRADING B-M	APPROX. 226.00± LB/S.Y.
⑤	BITUMINOUS MATERIAL FOR PRIME COAT (PC) - AGGREGATE FOR COVER MATERIAL (PC) ITEM 402-01; AT A RATE OF 0.30-0.35 GAL/SQ YD ITEM 402-02; AT A RATE OF 8-12 LB/SQ YD	
⑥	SOIL-CEMENT BASE @ 8.00" THICK ITEM 304-01.04 PROCESSING (RECLAIMED BASE MATERIAL); AT A RATE PER SY ITEM 304-01.08 PORTLAND CEMENT (FULL DEPTH PAVEMENT RECLAMATION); AT RATE OF 52 LB/SY	



D1 TYPICAL SECTION

D4 HEAVY-DUTY PAVEMENT

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

PAVEMENT MARKINGS

FINAL PAVEMENT MARKING

- (1) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.08, PAINTED PAVEMENT MARKING (PARKING LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

RESURFACING

- (2) WHERE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (3) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (4) THE CONTRACTOR SHALL BRING ALL HAND HOLES, MANHOLES, DRAINAGE INLETS, AND OTHER UTILITY ACCESS POINTS UP TO FINISHED GRADE. COST FOR UTILITY ADJUSTMENT TO BE INCLUDED IN THE PRICE BID FOR ITEM 415-01.02, COLD PLANING BITUMINOUS PAVEMENT.

EROSION PREVENTION AND SEDIMENT CONTROL

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT BARTLETT CITY SCHOOLS AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

NATURAL RESOURCES

- (9) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (10) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.

INSPECTION, MAINTENANCE & REPAIR

- (1) THE CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (2) CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (3) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE EPSC INSPECTION REPORT.
- (4) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (5) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (6) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (7) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (8) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

EROSION PREVENTION

- (9) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (10) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (11) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.
- (12) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.

- (13) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (14) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (15) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (16) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

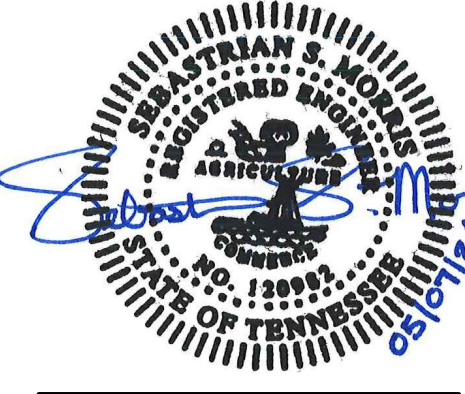
PERMITS, PLANS & RECORDS

- (17) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE BARTLETT CITY SCHOOLS SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE BARTLETT CITY SCHOOLS SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (18) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR, THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (19) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (20) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (21) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (22) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (23) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (24) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (25) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (26) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (27) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (28) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (29) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (30) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

D1	GENERAL NOTES
NTS	



PROJECT NO.	DATE
24218	MAY 7, 2024
DRAWN	CHECKED
SWJ	SSM

GENERAL NOTES (CONT.)

SUPPORT ACTIVITIES

- (1) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (2) MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (3) IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE CITY OF BARTLETT FOR REVIEW.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (4) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (5) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (6) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (7) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (8) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (9) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (10) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (11) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE CITY OF BARTLETT. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (12) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE CITY OF BARTLETT PRIOR TO STORING 1320 GALLONS ON SITE.

SPECIAL NOTES

RESURFACING

- (1) PRIOR TO RESURFACING, THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. VEGETATION SHOULD BE REMOVED FROM TRAVEL LANE OR SHOULDERS
 - b. GARBAGE AND CONSTRUCTION DEBRIS MATERIAL SHALL BE PICKED UP AND REMOVED.
 - c. USE A PROPANE TORCH TO BURN VEGETATION/WEEDES AND DESTROY SEED WITHIN THE CRACK
 - d. MILL EXISTING SURFACE 1.25" AND REPAIR TRAVEL LANE WITH ITEM NO. 307-01.01
 - e. AREA TO BE RESURFACED SHALL BE BROOMED TO REMOVE GRITS AND DEBRIS
 - f. THE COST FOR THE PREPARATION WORK (ITEMS a , b, c and e) ARE INCIDENTAL TO PAY ITEM 411-01.10.
- (1) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:
 - a. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
 - b. THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.
 - c. THE MILLED SURFACE SHALL BE PAVED WITHIN 96 HOURS .
 - d. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
 - e. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
 - f. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

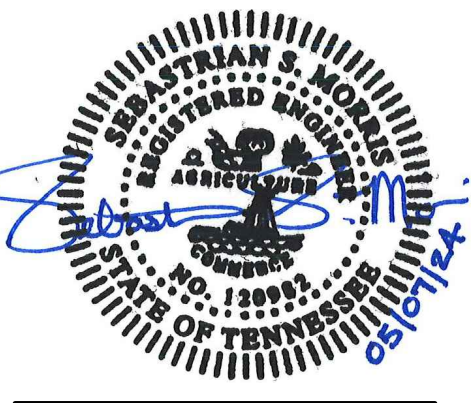
D1

NTS

SPECIAL NOTES

A2H ENGINEERS
ARCHITECTS
PLANNERS

A2H, INC.
3009 DAVIES PLANTATION ROAD
LAKELAND, TN 38002
P. 901.372.0404
WWW.A2H.COM



B BARTLETT
CITY SCHOOLS

BARTLETT CITY
SCHOOLS

PAVEMENT
FULL DEPTH
RECLAMATION

5705 STAGE ROAD
BARTLETT, TN 38134

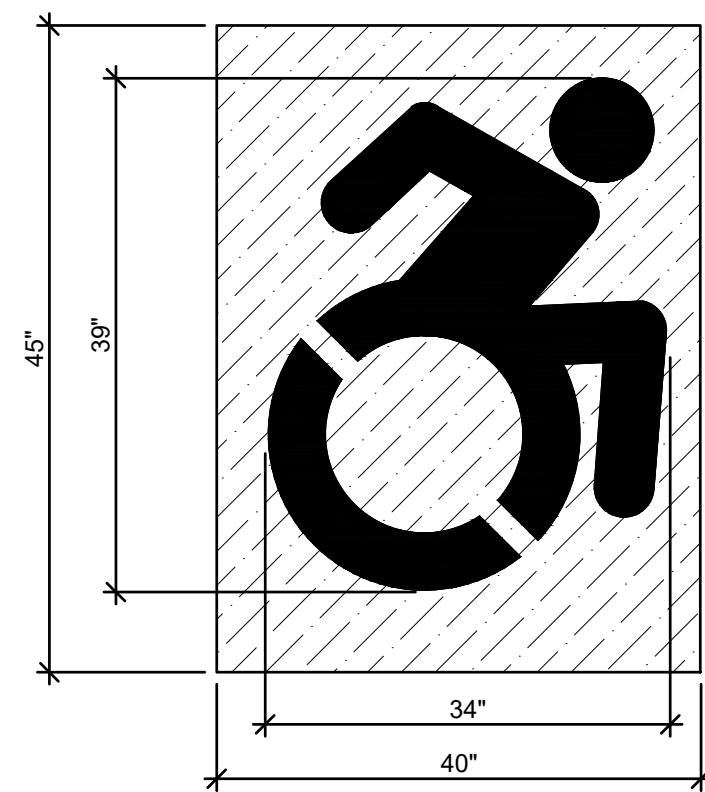
CONSTRUCTION
DOCUMENTS

REVISIONS

PROJECT NO.	DATE
24218	MAY 7, 2024
DRAWN	CHECKED
SWJ	SSM

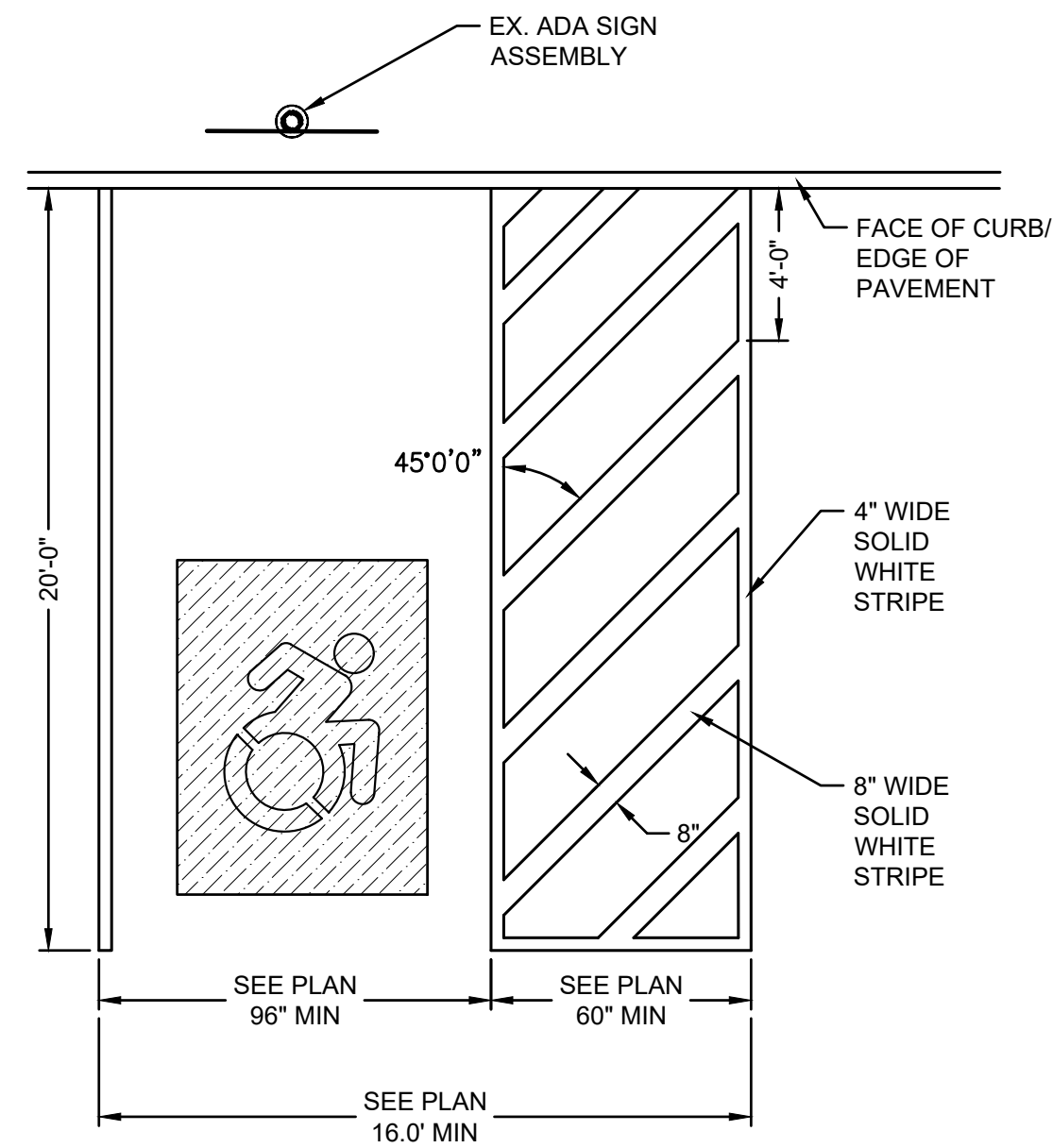
SPECIAL NOTES

2B

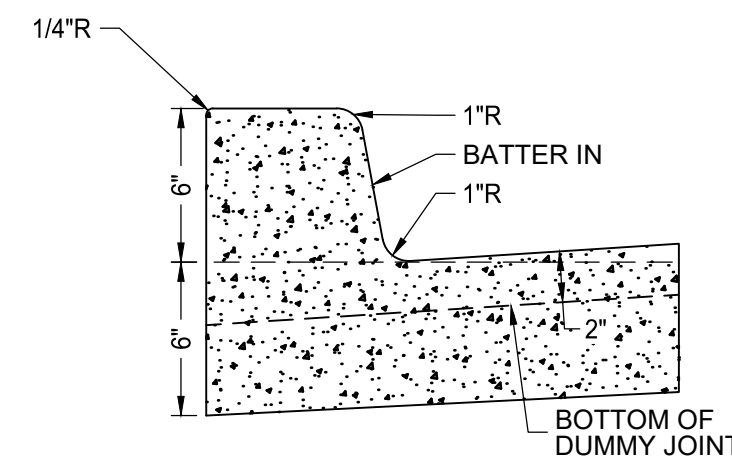


- NOTE:
 1. SYMBOL TO BE 4" WIDE WHITE TRAFFIC PAINT ON BLUE FIELD ON PAVEMENT. TYPICAL AT ALL ACCESSIBLE STALLS.
 2. VERIFY SIGNAGE WAUTHORITIES PRIOR TO INSTALLATION AND INSTALL APPROVED SIGNS ONLY

B1 ACCESSIBLE PARKING SYMBOL
 NTS

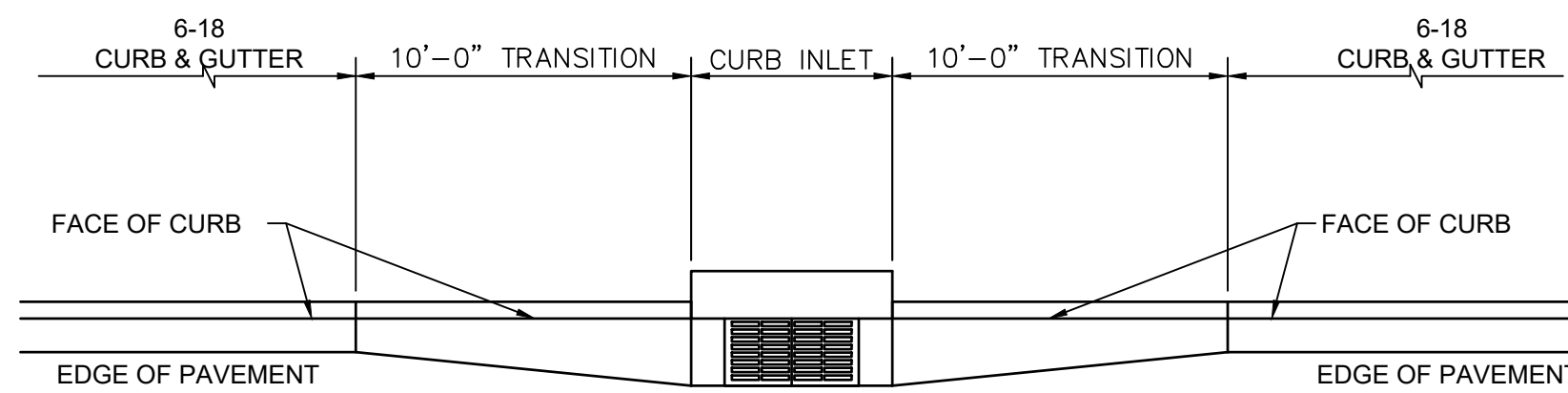


B2 ACCESSIBLE PARKING STALL
 NTS



- NOTES:
 1. DUMMY JOINTS WILL BE MADE AT 10' INTERVALS
 2. EXPANSION JOINTS WILL BE MADE AT 30' INTERVALS
 3. PRECUT JOINT MATERIAL TEMPLATES WILL BE USED IN ALL EXPANSION JOINTS
 4. PAY ITEM #702-01

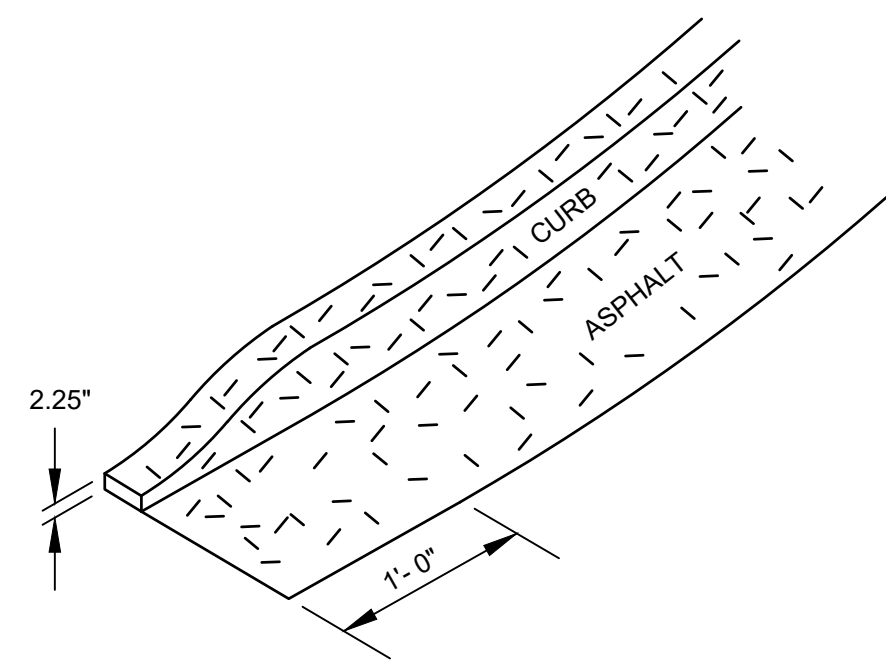
A3 6-18 CURB AND GUTTER
 NTS



B3 CURB TRANSITION DETAIL
 NTS

- LEGEND
 DSYL - DOUBLE SOLID YELLOW LINE
 SBWL - SINGLE BROKEN WHITE LINE
 SSL - SINGLE SOLID WHITE LINE
 HWL - HASHED WHITE LINE
 UNLESS OTHERWISE NOTED ALL STRIPING TO BE 4" WIDE.

C1 PAVEMENT MARKING LEGEND
 N.T.S.

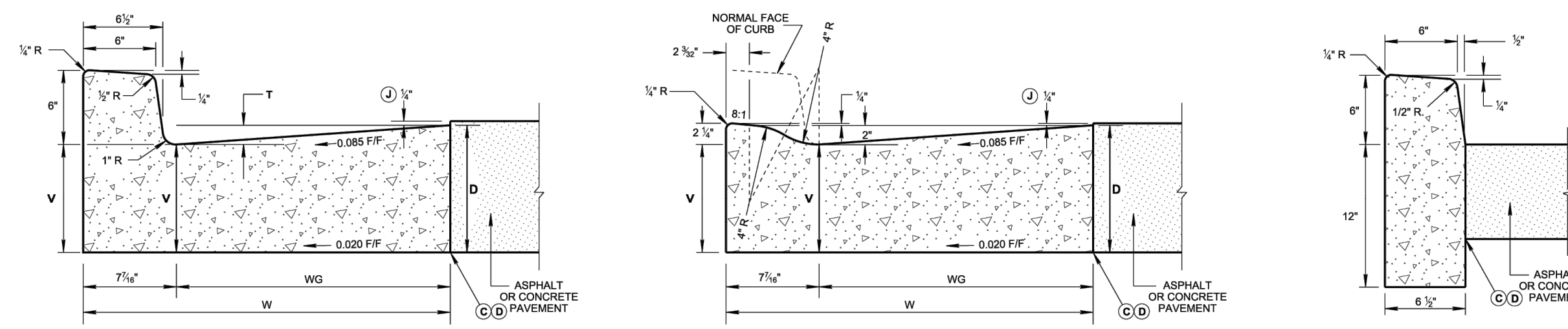


NOTE:
 TO BE CONSTRUCTED AT ALL TERMINATION POINTS OF CURB & GUTTER

D1 CURB END DETAIL
 NTS

D2 TDOT - RP-VC-10
 NTS

3/10/2021 8:37:32 AM P:\StandDraw\DESIGN STANDARDS\Standards Drawings - CURRENT\In Progress\110-104.08 Roadway - Pavement Appurtenances and Face (P) 104.08 Curbs (P)RPVC10-202102



TYPE	TOTAL WIDTH (W) IN INCHES	WIDTH OF GUTTER (WG) IN INCHES	VERTICAL DROP (T) IN INCHES	VERTICAL DEPTH (V) OF GUTTER * AT FLOW LINE
6-30	30	22 1/4"	2	D - 1 1/2"
6-36	36	28 3/4"	2 1/2	D - 1 3/4"

* VERTICAL DEPTH (V) MUST ALWAYS EXCEED SIX (6) INCHES.

DEPTH (D) OF GUTTER IN INCHES	TOTAL WIDTH (W) IN INCHES	6" CONCRETE CURB AND GUTTER (CY/LF)	LOWERED CONCRETE CURB AND GUTTER (CY/LF)
8	30	0.06469	0.05711
9	30	0.07780	0.07585
10	30	0.07151	0.06483
11	30	0.08706	0.08011
12	30	0.07953	0.07554
13	36	0.09632	0.08934
14	30	0.08724	0.08028
15	36	0.10558	0.09860
16	30	0.09486	0.08799
17	36	0.11484	0.10786

- LEGEND
 D = VERTICAL DEPTH OF GUTTER (AS BASED ON PAVEMENT DESIGN)
 T = VERTICAL DROP IN GUTTER FROM FRONT EDGE TO FACE OF CURB
 V = VERTICAL DEPTH OF GUTTER AT FLOW LINE
 W = TOTAL WIDTH OF COMBINED CURB AND GUTTER
 WG = WIDTH OF GUTTER

HEIGHT OF CURB	CUBIC YARD PER LINEAR FOOT
6"	0.02960
LOWERED CURB	0.02534

- GENERAL NOTES**
- FOR SPECIFICATIONS SEE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION, SECTION 702 - CEMENT CONCRETE CURB, GUTTER AND COMBINED CURB AND GUTTER.
 - THE FRONT FACE OF THE CONCRETE CURBS FOR ALL DEGREES OF CURVATURE SHALL CONFORM TO THE CONTOUR OF CURVATURE AND NO CHORD SECTIONS WILL BE PERMITTED.
 - CONCRETE EXPANSION JOINT MATERIAL IS TO BE 1/2" IN THICKNESS AT ALL LOCATIONS. ALL MATERIAL IS TO BE PRE-MOLDED FIBER IN ACCORDANCE WITH SECTION 905 - JOINT MATERIALS OF THE STANDARD SPECIFICATIONS.
 - EXPANSION JOINTS ARE TO BE PLACED AS FOLLOWS:
 - AT TANGENT POINTS OF CIRCULAR CURBS.
 - BETWEEN CURBS AND ADJUTING ROAD OBJECTS.
 - AT OTHER PLACES WHERE STRESSES MAY DEVELOP.
 - TO LINE UP WITH PAVEMENT JOINTS WHERE THE ADJACENT PAVEMENT IS CONCRETE.
 - THE MAXIMUM SPACING IS TO BE 100 FEET.
 - BETWEEN CONCRETE CURBS OR CURB AND GUTTERS AND CONCRETE PAVEMENT, IT IS NOT REQUIRED WHEN CURBS OR CURB AND GUTTERS ARE ADJACENT TO ASPHALT.
 - CONTRACTION JOINTS ARE TO BE SPACED AT 10 FEET. THE SPACING OF 10 FEET MAY BE REDUCED FOR CLOSURES, BUT NOT LESS THAN 6 FEET.
 - EDGES OF JOINTS SHALL BE FINISHED ON 1/4" RADIUS.
 - ALL COST OF JOINTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CONCRETE CURBS AND CONCRETE CURBS AND GUTTERS.
 - THE UNIT PRICE BID FOR CONCRETE CURB, CONCRETE CURB AND GUTTER AND CONCRETE PAVEMENT WILL INCLUDE ANY CIRCULAR SECTION REQUIRED TO BE BUILT CONFORMING TO SECTIONS SHOWN ON THIS SHEET.
 - PAYMENT WILL BE AS FOLLOWS:

ITEM NO. 702-01, CONCRETE CURB,	PER C.Y.
ITEM NO. 702-03, CONCRETE COMBINED CURB AND GUTTER,	PER C.Y.
 - THE 1/4" HEIGHT IS FOR WHEN ASPHALT PAVEMENT IS ADJACENT TO CONCRETE CURB AND GUTTER ONLY. CONCRETE PAVEMENT WILL BE FLUSH WITH THE TOP OF THE GUTTER.
 - LOWERED CONCRETE CURB NOTES
 - TO BE BUILT AS COMBINED CURB AND GUTTER, DETACHED CURB OR INTEGRAL CURB AS NOTED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
 - FOR DETACHED CURB, OMIT RADIUS AT FLOW LINE.
 - TO BE USED FOR PROPOSED RAMP OPENINGS.

REV 08-28-19 REVISED DETAILS FOR LOWERED CONCRETE CURB, 6" DETACHED CONCRETE CURB AND IF CONCRETE COMBINED CURB AND GUTTER. GENERAL NOTE: REVISED NOTES (A) AND (B) ADDED NOTE (C), MOVED LOWERED CONCRETE CURB NOTES (D), (E), (F), (G), (H) & (I). ADJUSTED WIDTH OF GUTTER (W) DIMENSION. REVISION SHEET.

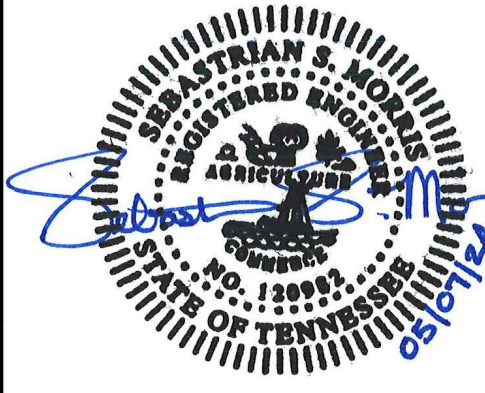
REV 03-04-21 REVISED THE DESCRIPTION OF THE STANDARD DRAWING. REMOVED TYPE 8-4 FROM THE DRAWING AND REVISED TABLES. REMOVED TOMBING FROM THE TITLE AND FROM THE TABLES. ADDED PAY ITEM NO. ON THE DRAWING.

(Replaces Std. Draw RP-NAC-10)

STATE OF TENNESSEE
 STANDARD DRAWING
 DEPARTMENT OF TRANSPORTATION

VERTICAL CONCRETE CURB AND GUTTER (FOR 8" TO 12" GUTTER DEPTH)

05-15-2018 RP-VC-10



BARTLETT CITY SCHOOLS

PAVEMENT FULL DEPTH RECLAMATION

5705 STAGE ROAD
 BARTLETT, TN 38134

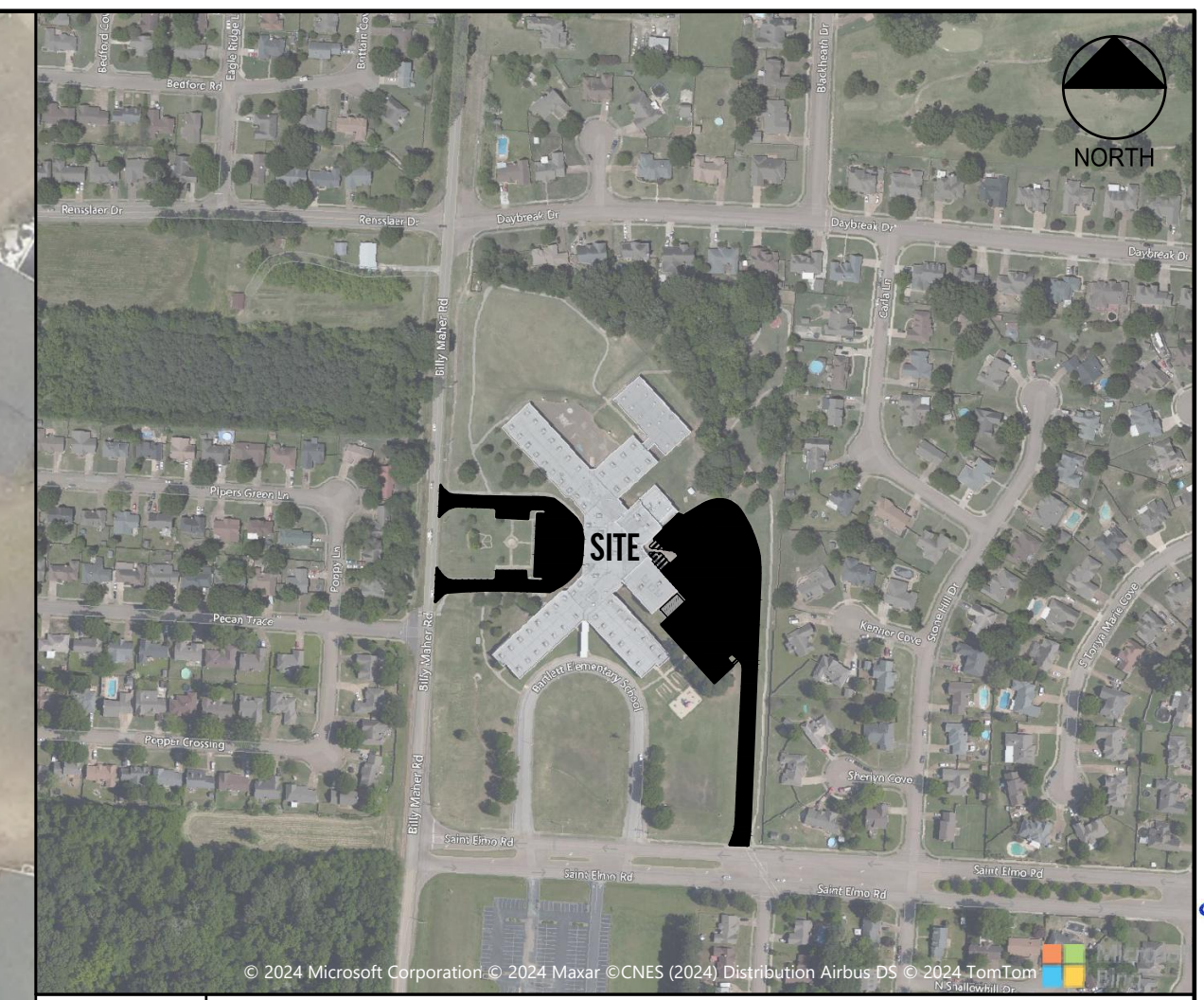
CONSTRUCTION DOCUMENTS

REVISIONS

PROJECT NO.	DATE
24218	MAY 7, 2024

DRAWN	CHECKED
SWJ	SSM

DETAILS



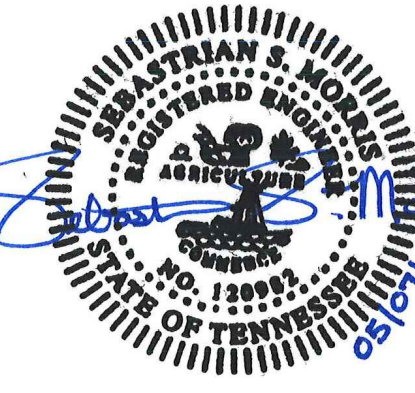
A5
NTS AREA MAP

D1
1"=40'

EXISTING CONDITIONS BARTELT ELEM.

GRAPHIC SCALE: 1" = 40'

NORTH



BARTLETT CITY SCHOOLS

PAVEMENT FULL DEPTH RECLAMATION

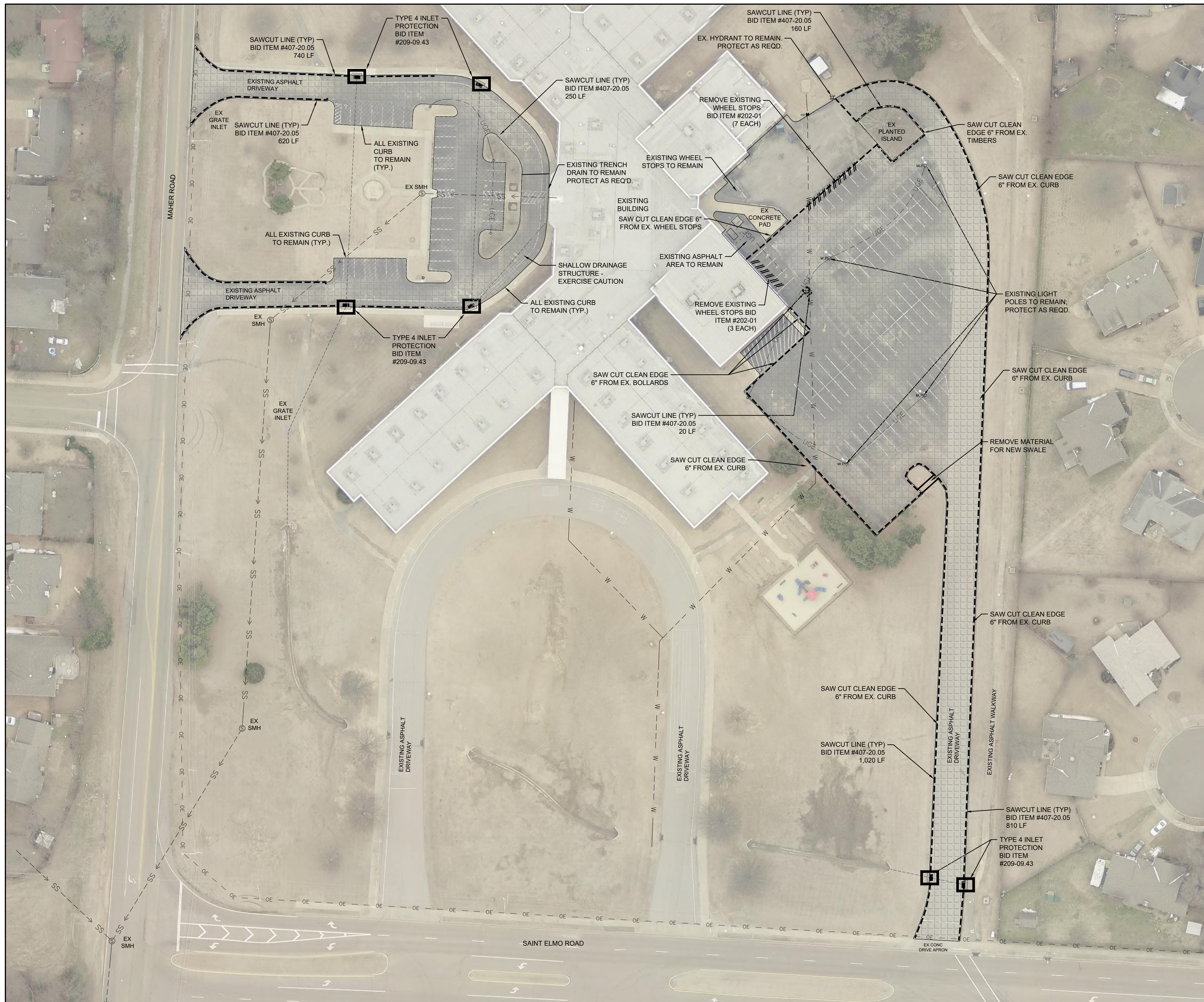
5705 STAGE ROAD
BARTLETT, TN 38134

CONSTRUCTION DOCUMENTS

REVISIONS

PROJECT NO. 24218	DATE MAY 7, 2024
DRAWN SWJ	CHECKED SSM

BARTLETT ELEM. EXISTING CONDITIONS

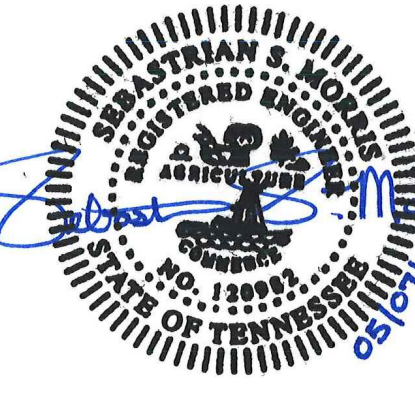


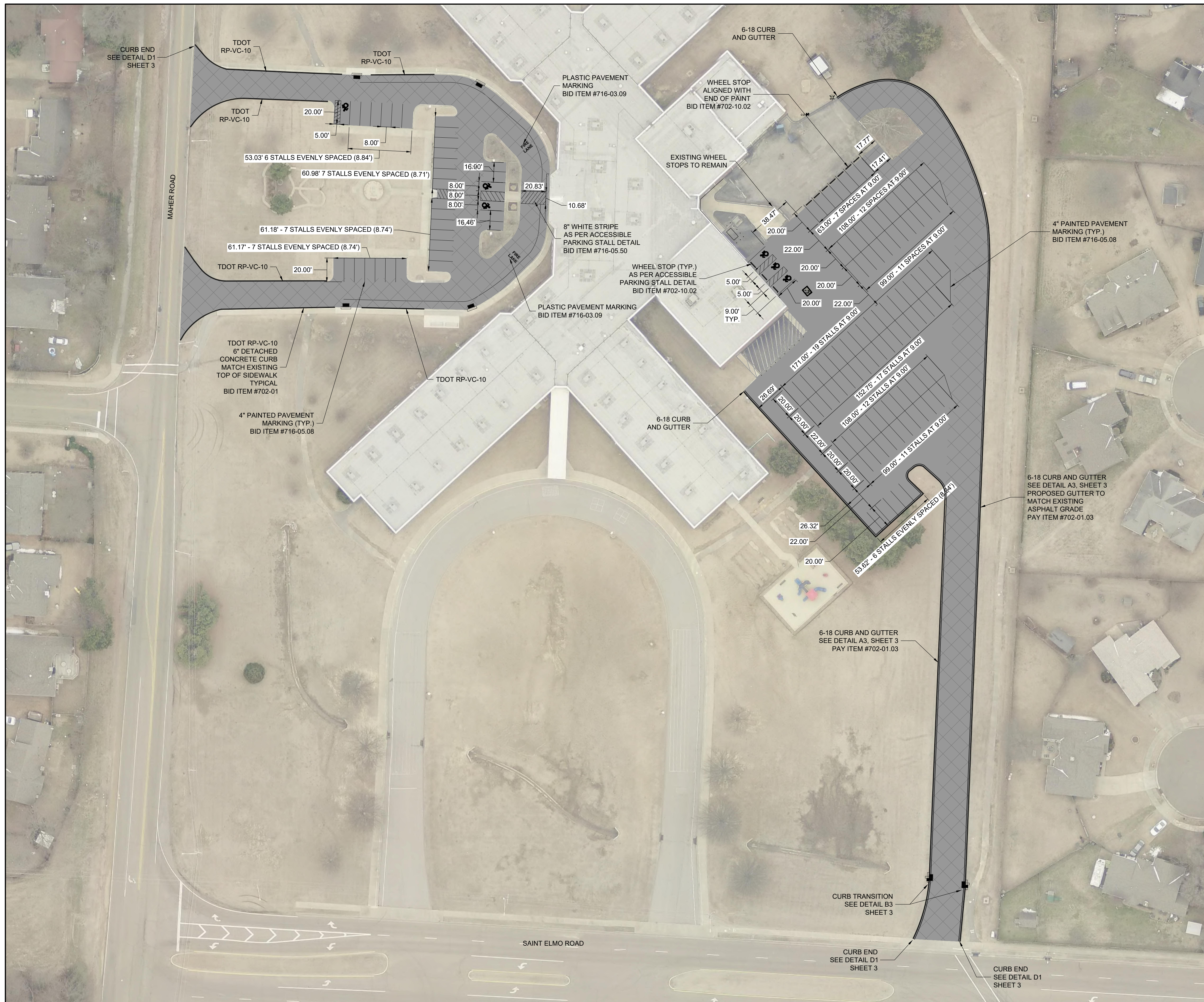
A5 AREA MAP
NTS

	MILL SURFACE COURSE - APPROX. 6,900 S.F. ITEM #415-01.02
	FULL-DEPTH RECLAMATION - APPROX. 82,000 S.F. ITEM #304-01.04 & #304-01.08
	SAW CUT CLEAN EDGE - APPROX. 3,775 L.F. ITEM 407-20.05
	REMOVE EXISTING WHEEL STOP - 10 EACH ITEM 202-01
	TYPE 4 INLET PROTECTION - 6 EACH ITEM #209-09.43

B5 DEMOLITION LEGEND
NTS

D1 DEMOLITION PLAN
1"=40'
GRAPHIC SCALE: 1" = 40'
NORTH





A5 AREA MAP
NTS

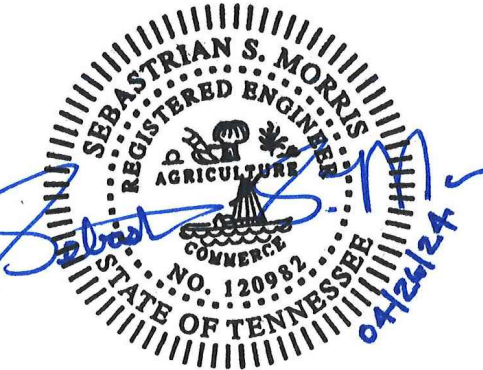
	RESURFACED AREA - APPROX. 88,750 S.F.
	HEAVY DUTY PAVING SECTION - APPROX. 45,050 S.F.
	4" PAINTED PARKING LINE - APPROX. 6,000 L.F. ITEM #716-05.08
	8" PAINTED PAVEMENT LINE - APPROX. 165 L.F. ITEM #716-05.50
	WHEEL STOP - 10 EACH ITEM #702-10.02
	PLASTIC PAVEMENT MARKING (HANDICAP SYMBOL) - 6 EACH ITEM #716-04.10
	FIRE LANE PLASTIC WORD PAVEMENT MARKING (FIRE LANE) - 2 EACH ITEM #716-03.09

B5 STRIPING LEGEND
NTS

D1 PROPOSED LAYOUT
1"=40'

GRAPHIC SCALE: 1" = 40'
0 40

NORTH



BARTLETT CITY SCHOOLS

PAVEMENT RESURFACING

5705 STAGE ROAD
BARTLETT, TN 38134

CONSTRUCTION DOCUMENTS

REVISIONS

PROJECT NO. 24218	DATE APRIL 26, 2024
DRAWN SWJ	CHECKED SSM

BARTLETT ELEM. PROPOSED LAYOUT